

TYPICAL PAVEMENT MARKINGS INTERSECTIONS WITHOUT CROSSWALKS

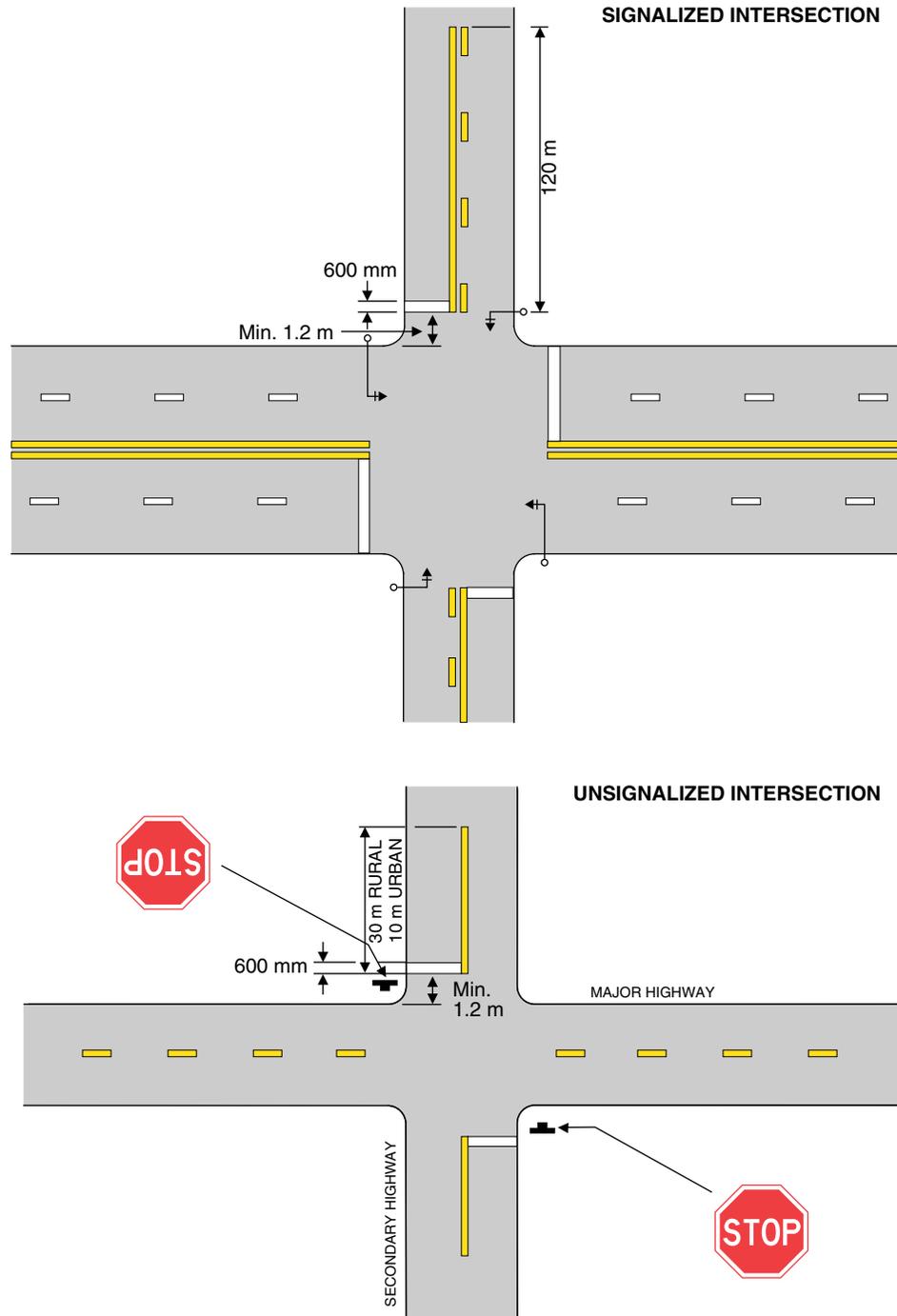


FIGURE C3-2

The Stop Line should extend from the right curb or pavement edge to the Directional Dividing Line or median island, if present, or, in the case of one-way streets, to the left curb or pavement edge.

Typical examples of Stop Line applications are illustrated in Figures C3-1 and C3-2.

C3.3 APPROACHES TO RAILWAY CROSSINGS

Pavement markings may be placed on a paved approach to a railway crossing where extra emphasis may be needed. These markings are not sufficient warning by themselves and must always be used in conjunction with signs and other devices. The markings must be white.

Typical pavement markings at a railway crossing are illustrated in Figure C1-6. The Stop Lines are perpendicular to the road centreline. The separation between the tracks and the Stop Lines is a minimum of 5 metres, measured perpendicularly to the nearest rail.

C3.4 GORE AREAS

Gore areas at the beginning of merging areas, at the end of diverging areas, and on the approach to objects within the roadway are marked as illustrated in Figure C3-3.

The borders of the gore areas are normal solid yellow lines where traffic passes the gore area in opposing directions and normal solid white lines where traffic in the same direction passes on both sides of the gore area. Wide solid lines should be used in locations where they have been used for adjacent markings.

Where traffic is allowed to pass on both sides of the object, the diverging lines extend from the lane line to points 300 mm clear of the edges of the object and a maximum of 3.0 m further, parallel to it.

Where vehicles are required to pass to the right of the obstruction, the diverging lines extend from the directional dividing line to points a minimum of 150 mm clear of the object. In addition, a second solid line is placed one line width to the right. It extends from a minimum of 30 m in advance of the beginning of the diverging lines and continues to the object. It may be extended further as an edge line parallel to the object and joined with the diverging line on the other side. This second solid line may be omitted on low-speed urban streets where the Directional Dividing Line is made a solid line for 30 m or more in advance of the approach marking.

The lengths of the diverging lines will depend upon the width of the obstruction and the speed of the approach traffic.

The interior of the gore area may be marked with diagonal lines to guide traffic away from the object, as described in Section C3.5 and illustrated in Figure C3-3.